

TOWNSHIP OF EAST BRUNSWICK

ORDINANCE NO: 24-04



ADOPTED: MARCH 25, 2024

AN ORDINANCE OF THE TOWNSHIP OF EAST BRUNSWICK ADOPTING AN AMENDED ROUTE 18 CORRIDOR REDEVELOPMENT PLAN DATED NOVEMBER 2023 IN ACCORDANCE WITH N.J.S.A. 40A:12A-7

WHEREAS, the Local Redevelopment and Housing Law, N.J.S.A. 40A:12A-1 et seq., as amended (the "Redevelopment Law"), provides a process for municipalities to participate in the redevelopment and improvement of areas in need of redevelopment; and

WHEREAS, the Township of East Brunswick ("Township") wishes to redevelop certain property identified as Block 6.02, Lots 6.20, 6.21, 6.22, 11.02, 12.01, 13.01, Block 6.01, Lots 15.02, 16.02, 16.03, 16.04; and Block 26.01, Lots 3, 4, 6, 8, 9.02, 10, 11 and 12, Township of East Brunswick, Middlesex County, New Jersey (the "Property") pursuant to the Redevelopment Law; and

WHEREAS, the Township designated EB Development Urban Renewal, LLC (the "Redeveloper") as the conditional redeveloper of the Property; and

WHEREAS, the Township's professionals and the Redeveloper have made significant progress in preparing an amended redevelopment plan which is consistent with the Redeveloper's amended concept plan for redevelopment of the Property; and

WHEREAS, the aforementioned amended redevelopment plan is entitled "Route 18 Corridor Redevelopment Plan for Block 6.02, Lots 6.20, 6.21, 6.22, 11.02, 12.01, 13.01, Block 6.01, Lots 15.02, 16.02, 16.03, 16.04; and Block 26.01, Lots 3, 4, 6, 8, 9.02, 10, 11 and 12, Township of East Brunswick, Middlesex County, New Jersey, Amended November 2023" (the "Amended Plan") prepared by DMR Architects and attached hereto as Exhibit A; and

WHEREAS, the Township Redevelopment Agency has reviewed the aforementioned Amended Plan and has recommended that the Township Council refer same to the Planning Board for review pursuant to N.J.S.A. 40A:12A-7(e); and

WHEREAS, pursuant to N.J.S.A. 40A:12A-7(e), prior to the adoption of a redevelopment plan, a planning board is required to transmit to the governing body, within 45 days after referral, a report containing its recommendation concerning a redevelopment plan; and

WHEREAS, on December 26, 2023, the Township adopted Resolution # 23-35 referring the Amended Plan to the Planning Board for review in accordance with the provisions of N.J.S.A. 40A:12A-7(e); and

WHEREAS, at its public meeting on January 24, 2024, the Planning Board adopted a Resolution recommending that the Amended Plan be adopted by Ordinance by the Township Governing Body.

NOW, THEREFORE, BE IT ORDAINED, by the Mayor and Township Council of the Township of East Brunswick, County of Middlesex, State of New Jersey, as follows:

Section 1. The Township hereby approves and establishes the Amended Plan, attached hereto as Exhibit A, for the Property in accordance with N.J.S.A. 40A:12A-7 of the Redevelopment Law; and

Section 2. The Amended Plan shall replace the current Route 18 Corridor Redevelopment Plan and shall supersede all provisions of the Zoning and Development Regulations of the Township of East Brunswick regulating the properties in the area addressed by the Amended Plan, unless otherwise noted in said Amended Plan; and

Section 3. Final adoption of said Amended Plan by the Township Council shall be considered an amendment of the Township of East Brunswick Zoning Map. The Zoning District Map in the Zoning Ordinances of the Township of East Brunswick shall be amended to include the boundaries described in the Amended Plan and the provisions therein; and

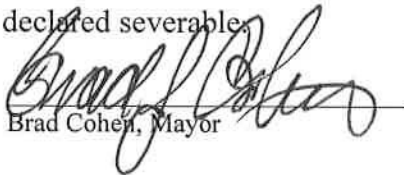
Section 4. Any provisions of Chapter 192, "Design Guidelines and Standards", shall apply so long as they are not superseded by or in direct conflict with the standards of said Amended Plan or with the intent of this Amended Plan. Any requirements or conditions not specifically identified in this Amended Plan shall revert to the Township of East Brunswick Zoning Ordinance. In the event of any inconsistencies between the provisions of the Amended Plan and any ordinance of the Township of East Brunswick, the provisions of the Amended Plan shall govern.

Section 5. Upon introduction and first reading, the Municipal Clerk is hereby directed to transmit a copy of this Ordinance by regular and certified mail to all record owners of property located within the Property and shall provide public notice of the second reading hearing date of this Ordinance; and

Section 6. A copy of this Ordinance and Amended Plan shall be forwarded to the Redevelopment Agency and Planning Board for their records; and

Section 7. Should any section, paragraph, sentence, or clause of this Ordinance be declared unconstitutional or invalid for any reasons, the remaining portions of this Ordinance shall not be affected thereby and shall remain in full force and effect, and, to this end, the provisions of this Ordinance are hereby declared severable.

Rejected
Approved



Brad Cohen, Mayor

3/26/24
Date

Reconsidered by Council _____

Override Vote Yes ____ No ____

DATE OF ADOPTION: March 25, 2024


Kevin McEvoy, Council President


Tamar Lawful, Municipal Clerk

RECORDED VOTE OF COUNCIL ON FINAL PASSAGE						
Council Member	Motion	Second	Yes	No	Abstain	Absent
Behal		X	X			
Wendell	X		X			
Winston			X			
Zimbicki			X			
McEvoy			X			

EXHIBIT A

[SEE ATTACHED]

Route 18 Corridor Redevelopment Plan



Block 6.02 Lots 6.20, 6.21, 6.22, 11.02, 12.01, 13.01, Block 6.01 Lots 15.02, 16.02, 16.03, 16.04; and Block 26.01
Lots 3, 4, 6, 8, 9.02, 10, 11 and 12;

**TOWNSHIP OF EAST BRUNSWICK,
MIDDLESEX COUNTY, NEW JERSEY**

Amended November 2023

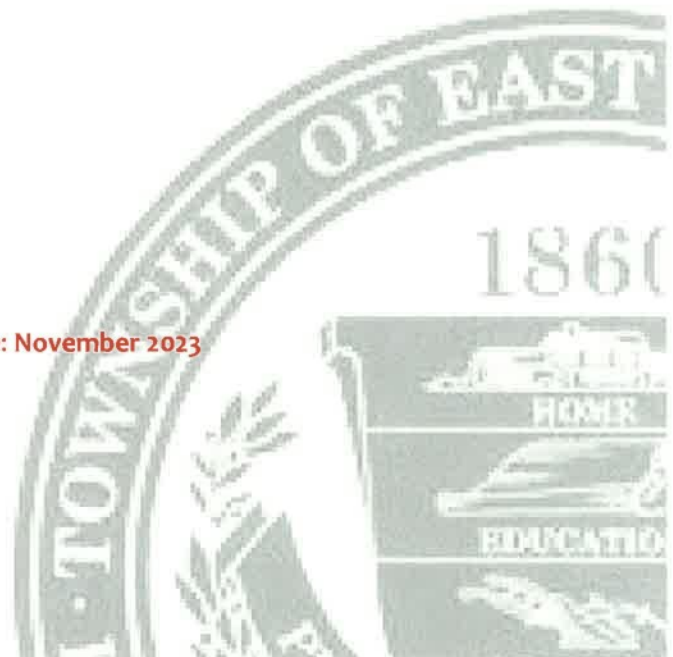
Prepared by:

DMR Architects
777 Terrace Avenue
Hasbrouck Heights, NJ 07604

A handwritten signature in black ink, appearing to read "Francis Reiner".

Francis Reiner, LLA - PP
License #: LI00616700

Date: November 2023



ACKNOWLEDGEMENTS

Township of East Brunswick Mayor and Council

Brad J. Cohen - Mayor
 Kevin McEvoy - Council President
 Dana Zimbicki - Council Vice President
 James Wendell - Member
 Dinesh Behal - Member
 Dana Winston - Member

Township of East Brunswick Government

Joseph Criscuolo- Township Administrator

Township of East Brunswick Redevelopment Agency

Mike Hughes - Executive Director
 Loren Morace - Redevelopment Agency Clerk
 James Wendell - Chairman
 Allison Nagelberg - Vice Chair
 Mirah Adah Becker, AICP/PP - Member
 Cindy Furgang - Member
 Dinesh Behal - Member
 Debbie Wynter - Member
 Rachel Cohen - Member

Township of East Brunswick Planning Board

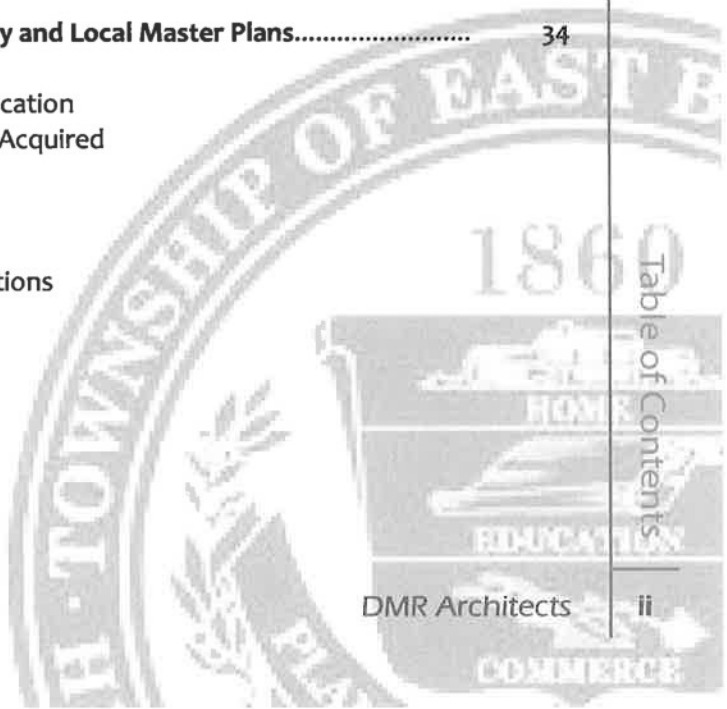
Shawn Taylor- Chair
 Charles Heppel - Vice Chair
 Adam Neary - Environmental Commission Representative
 Steve Phillips - ZBA Representative
 Kevin McEvoy- Council Representative
 Brad J. Cohen - Mayor
 Joseph Criscuolo- Township Administrator
 Muhammad Hashmi - Secretary
 Laurence Reiss - Member
 Erum Shakir - Alternate 1
 Mary Pan- Alternate 2

Planning Board Professionals

Keith T. Kipp - Director of Planning/Engineering
 Maryann Bucci Carter - Planner
 Jessica Tegeder - Secretary
 Anthony Abbonizio - Engineer
 Lawrence B. Sachs, Esq. - Attorney
 Jennifer Berger - Executive Administrative Assistant

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1.0: BACKGROUND INFORMATION

The Township of East Brunswick

The Township of East Brunswick is located in Middlesex County, the community is considered part of the New York City metropolitan area located on the southern shore of the Raritan River, adjacent to the city of New Brunswick. The town has a total of 2,270 square miles of land and located inland south of the Raritan River, it runs parallel to the South River on its easterly and southern side and is bordered by the Farrington Lake on its westerly side.

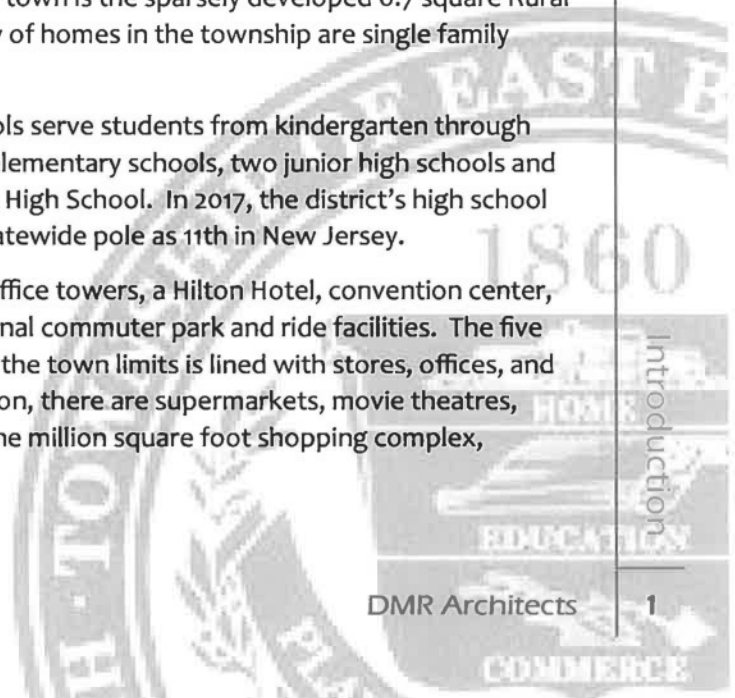
As of the 2010 Census, there were 16,810 households residing in the township. The population density was 2,189.6 per square mile. The racial makeup of the township was 69.36% White, 3.98% Black or African American, .10% Native American, 22.8% Asian, .01% Pacific Islander, 1.68% from other races. The Township's population was spread out with 24% under the age of 18, 7.3% from 18 to 24, 23% from 25-44, 32% from 45-64 and 13.5% who were 65 years of age or older. The average age was 42.5.

Originally a farming community, suburban settlement started in the 1930's with improved road access. The town is considered on of the fastest growing suburban areas in central Middlesex County. Large scale housing and road construction transformed East Brunswick into a more suburban community. The extension of the New Jersey Turnpike to East Brunswick in 1952 led to a sharp spike in population growth. According to the 2010 Census, the township's population was 47,512, a 1.6% increase form the 2000 Census.

East Brunswick has a variety of housing choices on the eastern side of town, ranging from single family homes, townhouses, condominium and apartment complexes. The western side of town is the sparsely developed 6.7 square Rural Preservation Zone. The majority of homes in the township are single family residences.

The East Brunswick Public Schools serve students from kindergarten through twelfth grade. There are eight elementary schools, two junior high schools and one high school, East Brunswick High School. In 2017, the district's high school was ranked by *Niche.com* in a statewide pole as 11th in New Jersey.

The Town Center includes two office towers, a Hilton Hotel, convention center, and one of the Township's regional commuter park and ride facilities. The five mile stretchy of Route 18 within the town limits is lined with stores, offices, and eating establishments. In addition, there are supermarkets, movie theatres, small shopping centers, and a one million square foot shopping complex, Brunswick Square Mall.



2.0: INTRODUCTION

2.1: BASIS FOR THE PLAN:

This redevelopment plan has been prepared for Block 6.02 Lots 6.20, 6.21, 6.22, 11.02, 12.01, 13.01, Block 6.01 Lots 15.02, 16.02, 16.03, 16.04 and Block 26.01 Lots 3, 4, 6, 8, 9.02, 10, 11 and 12. The site is located west of Route 18 between Ruth Street to the North and Lake Ave. to the south.

The Township of East Brunswick previously designated the areas in Image 1.2 as a non-condemnation area in need of redevelopment. The Township Council approved the recommendation and the area was designated. The Township Council determined portions of the area should be investigated again, this time the areas were investigated with condemnation.

In July 2017, the Township of East Brunswick with the assistance of Banisch Associates prepared an Investigation for Determination of a Condemnation Area in Need of Redevelopment in accordance with the criteria specified in the New Jersey Local Redevelopment and Housing Law (LRHL) at N.J.S.A. 40A:12A-6. The LRHL allows a municipality to prepare a redevelopment plan which provides the development regulations and other standards to guide future development for this area.

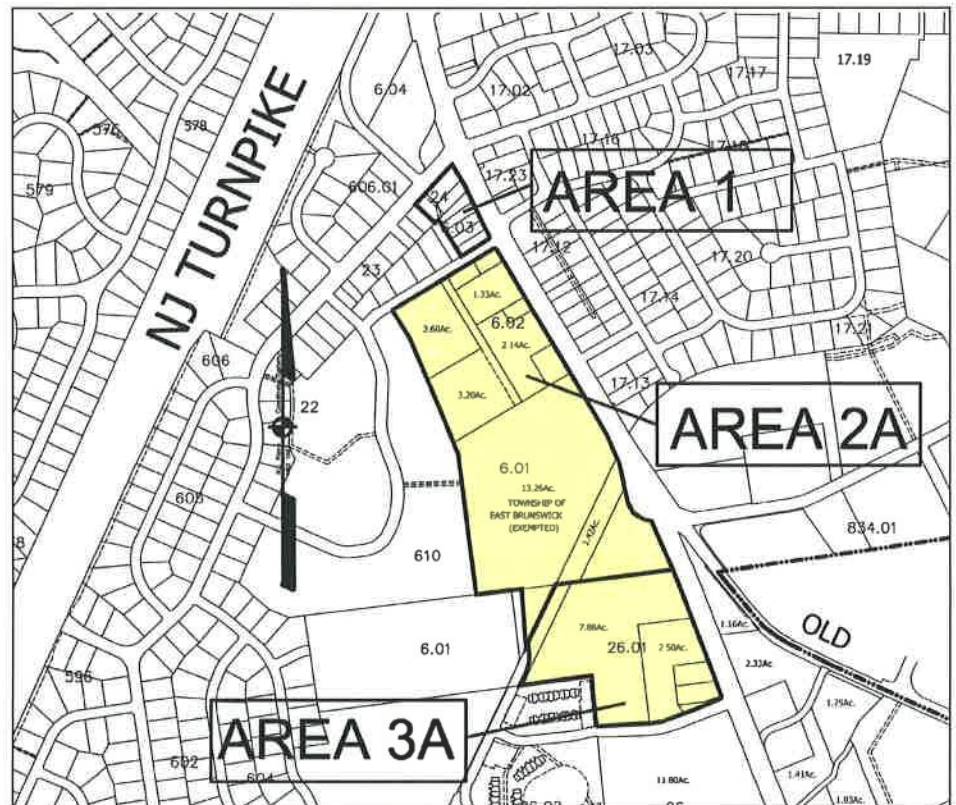
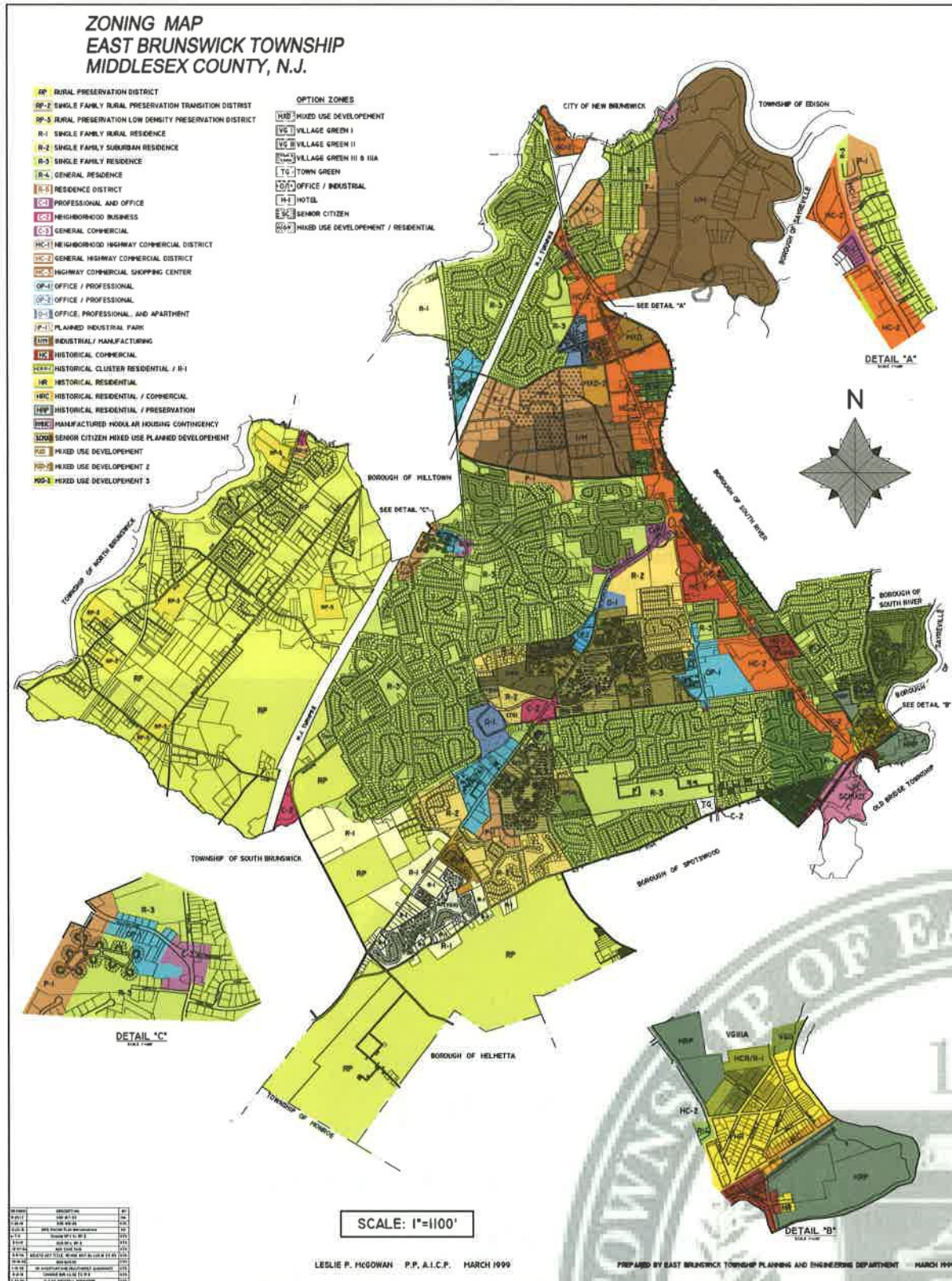


Figure 2.0: Image of Area Investigated for non-condemnation c/o Banisch Assocs. Report



Background Information

Figure 2.1: East Brunswick Township Zoning Map

2.2: SURROUNDING AREA CONTEXT:

The Development Area is located in the northern end of the Route 18 corridor. The surrounding properties are largely retail commercial in character. Many of the shopping areas have experienced a period of decline as newer shopping centers have developed. There are a number of the sites surrounding the area that are either vacant or under performing. Directly to the west of the Development Area is a residential development consisting of two-story residential buildings. To the east and across Route 18, there are a number of smaller professional office buildings.

2.3: PROPERTY INFORMATION:

All of the sites included in this Redevelopment Plan have previously been designated as Areas in Need of Redevelopment without condemnation. In July 2017, the Township Council decided some of the areas should be re-investigated as Areas in Need of Redevelopment with condemnation. The Township Council along with Banisch Associates studied the area and recommended it be designated. The Redevelopment Area (Area 2A & 3A) consists of approximately 88 acres of land located on Block 6.01 Lots 9.02, 15.02, 16.02, 16.03, 16.04, Block 6.02 Lots 6.20, 6.21, 6.22, 11.02, 12.01 and Block 26.01 Lots 3, 4, 6, 8, 10, 11, 12. The site is located in the HC-2, General Highway Commercial Zone.



Figure 2.2: Aerial Image of Redevelopment Area

2.4: REQUIRED COMPONENTS OF THE REDEVELOPMENT PLAN:

N.J.S.A 40A:12A-7 requires that a redevelopment plan include an outline for the planning, development, redevelopment, or rehabilitation of the Redevelopment Area sufficient to indicate the following:

1. Its relationship to definite local objectives as to appropriate land use, density of population and improved traffic and public transportation, public utilities, recreational and community facilities and other public improvements.
2. Proposed land uses and building requirements in the project area.
3. Adequate provisions for the temporary and permanent relocation, as necessary for residents in the project area, including an estimate of the extent of which decent, safe, and sanitary dwelling units affordable to displace residents will be available to them in the existing local housing market.
4. An identification of any property with the redevelopment area which is proposed to be acquired in accordance with the redevelopment plan.
5. Any significant relationship of the redevelopment plan to:
 - The master plans of contiguous municipalities;
 - The master plan of the county in which the municipality is located;
 - The State Development and Redevelopment Plan adopted pursuant to the "State Planning Act" P.L. 1985,c.398 (C.52:18A-196 et al.).
6. An inventory (as of the date of the adoption of the resolution finding the area to be in need of redevelopment) of all housing units affordable to low and moderate income households, as defined pursuant to N.J.S.A. 52:27D-304, that are to be removed as a result of implementation of the redevelopment plan, whether as a result of subsidies or market conditions, listed by affordability level, number of bedrooms, and tenure.
7. A plan for the provision, through new construction or substantial rehabilitation of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of the redevelopment plan .
8. The redevelopment plan may include the provision of affordable housing in accordance with the "Fair Housing Act," N.J.S.A. 52:27D-301 et seq. and the housing element of the municipal master plan.
9. The redevelopment plan shall describe its relationship to pertinent municipal development regulations as defined in the "Municipal Land Use Law," P.L.1975, c. 291 (C.40:55D-1 et seq.).
10. The redevelopment plan must state whether it shall supersede applicable provisions of the development regulations of the municipality or constitute an overlay zoning district within the redevelopment area.
11. All provisions of the redevelopment plan shall be either substantially consistent with the municipal master plan or designed to effectuate the master plan; but the municipal governing body may adopt a redevelopment plan which is inconsistent with or not designed to effectuate the master plan by affirmative vote of a majority of its full authorized membership with the reasons for so acting set forth in the redevelopment plan.

3.0: ZONING

3.1 Permitted Uses:

1. The following uses are permitted in the redevelopment area:
 - a. **Residential:** Multi-family, apartments, **conventional or stacked** townhomes, two over two, residential over retail/commercial, residential lining a parking structure and residential uses over parking structures;
 - b. **Commercial / Office:** General and medical office, physical therapy, basic research, outpatient care facilities, professional uses, banks, pharmacy, hotel, day spa permitted as freestanding structures or as a liner around or over a parking structure;
 - c. **Retail:** **Restaurants with drive thru**, eating and drinking establishments, cafes, shops, bakery, delicatessen, general stores, movie theater, grocery store / supermarket, book and stationery, florist, as freestanding structures or as a liner around parking structures;
 - d. **Entertainment:** Athletic and sports facilities, YMCA's, health clubs (public or private) rock climbing, fitness related uses and other social or business related activities;
 - e. **Civic, Cultural, Institutional and Religious:** Libraries, museums, theaters, art galleries, police and fire sub-stations, municipal and civic uses;
 - f. **Outdoor Open Markets:** Food trucks, ice skating, live performances, outdoor movies, outdoor markets and selling fresh food and plants based on state laws and regulations;
 - g. **Structured Parking:** Free standing or as an accessory use to a permitted use;
 - h. **Live Work / Studios:** For artists, designers, photographers, musicians, sculptors, gymnasts, potters, antique dealers and designers of ornamental and precious jewelry;
 - i. **Bus / Transit Stations:** Including parking facilities, "Park and Ride" and "Kiss and Ride";



Figure 3.0: Example of Mixed use residential over retail



Figure 3.1: Restaurant with outdoor dining facing a public plaza



Figure 3.2: Example of Residential townhomes and flats

- j. **Public Plaza, Park or Open Space:** Multi-functional outdoor uses including but not limited to music, movies, performances, ice skating, water, festivals, food trucks for individual or public gatherings;
- 2. Any use not specifically stated as a Permitted Use is not permitted in this Redevelopment Area.
- 3. For all single use free standing structures including but not limited to: Commercial, Retail, Civic, Higher Education and Transit Stations, see Section 4.0 Development / Place Making Design Standards;
- 4. Existing nonconforming uses and structures are permitted to continue as per N.J.S.A. 40:55D-68;
- 5. Refer to the Township of East Brunswick Zoning Ordinance for all applicable definitions;



Figure 3.4: Public open space

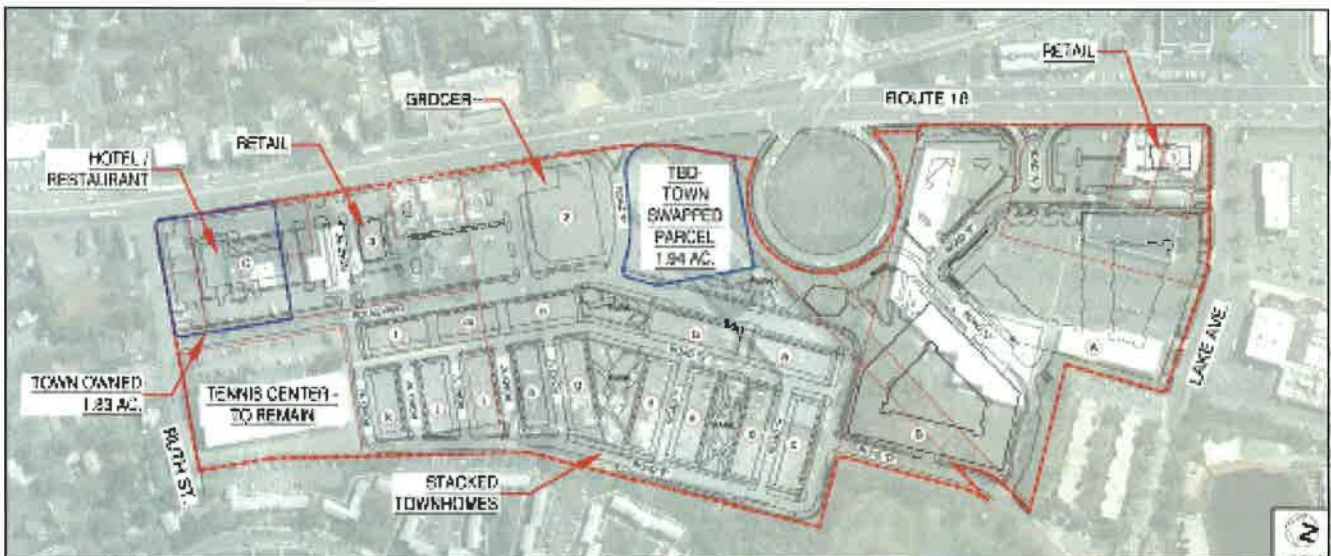


Figure 3.5: Regulating Land Use Plan

Use and Bulk Requirements

3.2 Accessory Uses:

1. Accessory structures and uses shall comply in all respects with the requirements of this ordinance applicable to the principal structures and uses.
 - a. No accessory structure shall be located closer to the street right-of-way line than the required front yard setback of the principal structure;
 - b. No portion of an accessory structure shall include living quarters except parking structures as an accessory use may contain residential over the structure;
 - c. Accessory structures below grade shall have a minimum rear or side yard, when not abutting a street, of five feet;
2. When an accessory structure is attached to the principal structure, it shall comply in all respects with the requirements of this ordinance applicable to the principal structure.
3. No accessory structure shall be constructed or placed on any lot unless the principal structure is first constructed or placed upon said lot, with the exception of parking facilities (surface or structure) which may be constructed prior to the primary use.
4. In no event shall the height of an accessory structure exceed the height of the principal building except for parking structures.
5. The aggregate area covered by accessory structures shall not exceed twenty-five percent (25%) of the rear yard, with the exception of parking structures exclusive of required buffer areas.
6. The following accessory uses shall be permitted within a permitted hotel, commercial or retail use provided they are located on the first or second floor.
 - a. Bank branch, walk-up ATM's;
 - b. Retail shops;
 - c. Barber, beauty and coffee shops;
 - d. Confectionery and tobacco sales;
 - e. Dry cleaning establishments;
 - f. Travel agencies and automobile rental services;
 - g. Conference center, meeting rooms;
7. Massage therapy is permitted as an accessory use;



Figure 3.6: Commercial use - Hotel



Figure 3.7: Accessory use - Conference Center



Figure 3.8: Accessory use - Spa



Figure 3.9: Accessory Use - Restaurant

3.3 Prohibited Uses:

Any use not specifically designated as a principal permitted use, an accessory use or a conditional use is specifically prohibited from this Redevelopment Plan. Prohibited uses include but are not limited to:

- Acetylene gas manufacture or storage;
- Adult day care centers;
- Adult entertainment / sales;
- Alcohol manufacture;
- Ammonia, chlorine or bleaching powder manufacture;
- Boarding houses, rooming housing, and half-way houses;
- Carousels, ferris wheels, merry-go-rounds, roller coasters, shooting galleries;
- Arsenals, storage or manufacture of gun powder, ammunition and blasting material;
- Asphalt manufacture or refining;
- Auction rooms; flea markets;
- Auto laundries, except when operated in conjunction with a duly authorized new car agency and when located on the same property;
- Automobile junk-yards or wrecking yards;
- Blast furnace, forge plants, rolling mills, foundry;
- Boiler works;
- Brick, pottery, tile or terra-cotta for manufacturing purposes;
- Celluloid or film manufacture or extraction of products there from;
- Chemical manufacture or storage of chemicals having a flammable, explosive or toxic base or which have an odor;
- Cement, cinder block, lime, gypsum or plaster of paris manufacture;
- Coke ovens;
- Community drop-in centers, homeless centers or rehabilitation centers for those afflicted with drug abuse alcoholism, disease, or mental illness;
- Cotton oil manufacture;
- Disinfectant, insecticide or poison manufacture;
- Distillation of coal, petroleum, refuse, grain, wood or bone;
- Dog pounds or animal shelters;
- Drive-thru's;
- Dye manufacture;
- Emery cloth and sandpaper manufacture;
- Establishments for the distribution of free food, toiletries, clothes, household goods, etc. including soup kitchens;
- Explosives or fireworks manufacture and storage;
- Fat-rendering plant;
- Fertilizer manufacture;
- Fish and meat: wholesale smoking or curing;
- Freak shows;
- Free standing news racks;
- Gas Station/Convenience Store;
- Glue, size, adhesive or gelatin manufacture;
- Grain drying or food manufacture from refuse, marsh or grain;
- House-wreckers yard or secondhand lumberyard;
- Incineration, except publicly owned incinerators or accessory to an apartment building or hospital, reduction, storage or dumping of slaughter house refuse, rancid fats, garbage, dead animals or offal;
- Junk-yards or the storage, sorting or bailing of junk, scrap iron, paper, bottles, metal or rags;
- Match manufacture;
- Motels, tourist camps or cabins; trailer camps or courts (Inns are permitted);
- Oilcloth and linoleum manufacture;
- Paint, oil, varnish, turpentine, shellac, enamel, japan, lacquer or solvents manufacture;
- Paper pulp manufacture;
- Pawn shops;
- Petroleum refining;
- Plasma center;
- Plastics manufacture or the manufacture of articles from plastic having an inflammable base;
- Potash works;
- Power forging, riveting, hammering, punching, chipping, drawing, rolling or tumbling of metals except as necessary incident of manufacture of which these processes form a minor part and which are carried on without objectionable noise audible beyond the limits of the lot;
- Printing ink manufacture;
- Quarry, sand-pits, gravel pits, topsoil stripping;
- Raw hides or skins storage, cleaning, curing, pickling or tanning or retaining;
- Rock or stone crusher;
- Rubber, latex or gutta-percha manufacture or treatment;
- Shoddy manufacture or wool scouring;
- Slaughtering or wholesaling of animals or fowl;
- Smelting, smelters, and foundries;
- Soap manufacture;
- Starch, glucose or dextrin manufacture;
- Steel furnace or rolling mill;
- Stockyards;
- Sugar refining;
- Sulphurous, sulfuric, acetic, nitric, picric, carbolic or hydrochloric acid manufacture;
- Tar distillation or manufacture;
- Tallow, grease or lard manufacture or refining;
- Tobacco manufacture or treatment;
- New or used automobile sales;
- Use Group H-High hazard use as defined in the Building Code;
- Yeast plant;
- Vape Shop;

3.4 Density, Area, Yard and Height Requirements:

The following requirements apply to the Redevelopment Area:

1. **Min. Lot Area:** The minimum lot area is 1.0 acres (43,560 sf);
2. **Town Center Zone:**
 - a. Residential: Max number of units: 400;
 - b. Retail: Min. / Max. square footage: 40,000 / 120,000 sf;
 - c. Hotel: Min / Max. number of rooms: 100 / 200 rooms;
Hotel can include ground floor retail and/or restaurants
 - d. Office: Min / Max square footage: 0 / 80,000 sf;
3. **Neighborhood / Highway Commercial Zone:**
 - a. Residential: Max number of units: 550 units;
 - b. Retail: Min. / Max. square footage: 10,000 / 40,000 sf;
 - c. Office: Min / Max square footage: 0 / 40,000 sf;
4. **Min. First Floor Height:** The minimum first floor height for any commercial / retail use is 14'-0" floor to floor;
5. **Max. Number of Phases:** Six (6) phases;
6. **Apartment Unit Sizes (excluding townhomes):**
 - a. A maximum of 10% can be under 550 sf;
 - b. A maximum of 15% can be between 550 sf and 650 sf;
 - c. A maximum of 25% can be over 1,250 sf;
7. **Min. Setback:**
 - a. The minimum setback for new construction along the proposed roadway is 16'-0" from the face of the curb;
 - This includes all access roads from Route 18 to the boulevard;
 - b. The minimum setback for new construction along Route 18 shall be 50'-0" from the face of curb with the exception of a grocer which shall have a minimum setback of 25'-0" from the existing curb along Route 18;
 - c. The minimum setback for all other roads shall be 12'-0" minimum;



Figure 3.12: Map of different zoning areas



Figure 3.10: Streetscape setback along ground floor retail



Figure 3.11: Mixed use with boulevard



Figure 3.13: Streetscape with street furniture zone

- d. **Other Setback Requirements:**
- Turn lanes are not included as a part of the setback;
 - The minimum setback applies to all public street frontages, not just to the street toward which the structure is oriented;
 - Entrances to garages are not considered frontages;
- e. Utility structures, exhaust air vents, backflow preventers, or other similar devices when located above grade, must be located behind the setback, be screened and should not be located on the Boulevard;
- Utility structures located below grade may be located within the setback and on the Boulevard;
8. **Min. Side / Rear Yard:**
- a. The minimum rear / side setback is 20'-0" to all exterior property lines;
- Interior property lines and townhomes are not included in this requirement (0 feet is permitted for all interior property lines);
9. **Min. Stories/Bldg. Height for all Mixed Use Bldgs:**
- a. Mixed Use Buildings: Minimum 3 st. (38'-0");
- b. All proposed buildings that front the boulevard are required to be a minimum of 3 stories;
- Public / Civic Buildings do not need to meet this requirement;
- c. Height is measured from the average of the top of curb of the two corners of the proposed building. For projects located at a corner lot, the height will be measured from the top of curb of the four corners located along the primary building facade;
- d. See Section 3.0 Development / Place-Making Design Standards for all building design requirements;
10. **Min. Number of Stories/Bldg. Height for all other Bldgs:**
- a. Freestanding Commercial / Retail Buildings: 1 st. (16'-0");
- b. Townhomes: 2 st. (24'-0");
- c. Hotel / Office: 2 st. (24'-0");
- d. Civic / Municipal: 1 st. (16'-0");
11. **Public Plaza:**
- a. A minimum 16,000 sf multi-functional public plaza shall be located along the main Boulevard outside of the public right of way; (See Design Standards for all applicable requirements)



Figure 3.14: Public plaza / open space with multi-functional programming



Figure 3.15: Conceptual site plan depicting a mixed use town center with active first floor retail uses. Includes a public plaza/open space, hotel, grocer and townhomes



Figure 3.16: Conceptual ground level rendering of the Phase 1 building with retail .



Figure 3.17: Phase 1 outdoor amenity.



Figure 3.18: Conceptual ground level streetscape rendering

Use and Bulk Requirements

3.5 - Parking Standards:

1. Parking Standards:

The minimum parking requirements for the Redevelopment Area are as follows:

<u>Use</u>	<u>Parking Ratio</u>
- Multi-family (Studio & One Bedroom):	1.0 sp per dwelling unit
- Multi-family (2 and 3 bedrooms):	1.5 sp per dwelling unit
- Townhomes:	2.0 sp per dwelling unit
- Retail:	4.0 sp / 1,000 GFA*
- Restaurant:	6.0 sp / 1,000 GFA*
- Medical Office:	4.0 sp / 1,000 GFA***
- General Office:	3.5 sp / 1,000 GFA
- Civic, Cultural, Institutional:	1.0 sp / 4.0 seats
- Assembly:	2.5 sp / 10.0 seats
- Hotel:	0.75 sp / per room + 1 sp / employee**

Notes:

- When the formula or parking spaces required results in a fraction of a space exceeding 0.49, a full space shall be required;
 - Gross Floor Area is the total interior floor area of all floors determined by measuring the inside dimension of the outside walls of the structure;
 - Up to 10% of the required parking stalls may be designated for compact cars;
 - Tandem parking is prohibited;
 - Outdoor seating areas do not count toward the required parking ratios(*);
 - Accessory uses do not require parking;
 - All surface parking shall be screened based on the Design Standards indicated in the Streetscape section of this Redevelopment Plan;
 - For Civic Uses without seating, the Retail/Commercial parking requirements shall apply;
 - Employee parking is based on the number of employees per shift (**);
 - For projects where medical office use comprises over 25% of office space in the project the parking requirements shall be 6.0 spaces per 1,000 GFA(***);
- Setback Maneuvering:** No residential parking or maneuvering space is permitted within a required front yard setback, or between the permitted use and the required setback, except driveways providing access to the parking area may be installed across these areas. Commercial/Retail parking and maneuvering space is permitted within a required front yard setback as long as it is designed not to interfere with traffic exiting the public roadway.
 - Shared Parking:** Joint use of up to 50 percent of required parking spaces for the secondary use only, may be permitted for two or more uses located on the same parcel (on-street parking does not count toward this requirement), provided that the developer can demonstrate the uses will not substantially overlap in hours of

operation or in demand for the shared spaces.

Any shared parking shall require a Shared Parking Analysis based on the ULI Shared Parking Software or a comparable software model, and shall be prepared by a credible expert, such as an experienced parking or land use consultant, planner, architect or engineer.

4. **Existing Non-Conforming Parking:** Shall be removed if the property undergoes a change of use or expansion. However, no additional parking is required for the reuse of an existing building, or due to the removal of non-conforming parking.
5. **On-Street Parking Spaces:** Parking spaces located along the portion of a public street(s) abutting the use may be counted toward the minimum number of parking spaces required for retail / commercial uses only. On-street parking spaces must be located on the same side(s) of the street as the use. On-street parking cannot be counted for shared parking.
6. **Bicycle Parking:** Bicycle parking is required for new developments at one bicycle parking space for every 50 vehicle parking spaces, up to 200 vehicle parking spaces.
 - a. Thereafter, one bicycle parking space shall be provided for every 100 vehicle parking spaces. Fractions equal to or greater than one half resulting from this calculation shall be considered to be one bicycle space;
 - b. Bicycle parking shall not be located within the building setback;
7. **Structured/Underground Parking Decks:** For residential units the following requirements shall be met:
 - a. Parking Security / Gates: Residential developments, including mixed or multi-use with a residential component can provide security for residents by controlling vehicular and pedestrian access to the second level or underground level in areas designated for the residential parking;
 - b. A portion of at-grade parking areas shall remain available for commercial, retail, office, and residential uses;
9. **Handicapped Accessible Parking Requirements:** Refer to Handicapped Parking Schedule in the City of East Brunswick Zoning Ordinance;
10. **Additional Off-Street Parking Regulations:** For all parking standards not covered in this section refer to Off-Street Parking Regulations in the City of East Brunswick Zoning Ordinance;
 - a. Off-street parking dimensions can be 9'-0" x 18'-0" and allow two lane drive aisle dimensions can be 24'-0";
 - b. A maximum of 10% can be compact parking with minimum dimensions of 8'-0" wide by 16'-0" long;
11. **Electric Vehicle Charging Stations:** 1 space per 100 dwelling units;

3.6 Screening Standards:

The provisions of this section shall be met at the time land is developed or the land and structures are redeveloped. The requirements of this section do not apply to lots or portions of lots, which are undeveloped.

1. **Off Street Parking:** All proposed off street surface parking areas with twenty (20) spaces or more, shall be screened from all public streets with the following criteria:
 - a. A four foot (4'-0") minimum planting strip shall be located between the back of the public sidewalk and the parking area;
 - b. The planting strip shall be planted with evergreen shrubs at least three feet high (3'-0") at the time of planting with species that will form a year round dense screen;
 - The maximum height for planting should be maintained at no greater than 4'-0" in height;
 - c. Perimeter trees shall be planted at no greater than thirty foot (30'-0") on center based on the perimeter length of the parking area;
2. **Interior Landscape Planting:** All open parking areas of 36 or more parking spaces or at least 12,000 square feet in area shall provide at least one 4" caliper tree for every eight parking spaces which include perimeter trees; (See Section 3: Streetscape Standards for minimum street tree requirements.)
 - a. Fractions equal to or greater than one half resulting from this calculation shall be considered to be one tree;
 - b. Each such tree shall be located in a planting island with a minimum area of 130 square feet of pervious surface;
 - c. The following distribution of trees shall apply:



Figure 3.19: Conceptual streetscape rendering.

- Each end space in a row of twelve or more parking spaces shall fully abut a planting island or a perimeter landscaped area along the long dimension of the end space;
- No more than 20 parking spaces shall be permitted between planting islands, or a planting island and a perimeter landscaped area;

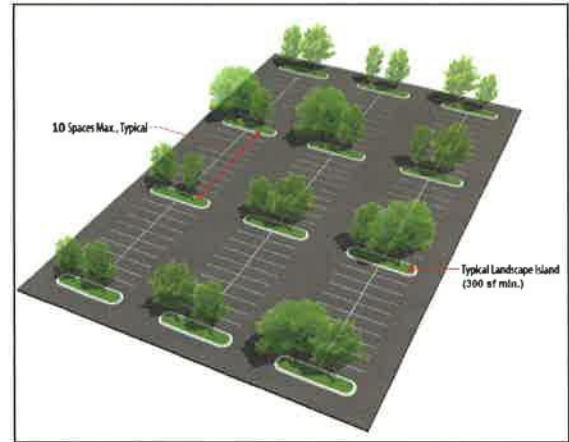


Figure 3.20: Screening requirements for surface parking

3. **Required Screening:** The following uses must be screened from abutting property and view from a public street:
 - a. Dumpsters, recycling containers (except for recycling containers located at recycling collection centers), or solid waste handling areas;
 - b. Service entrances or utility structures associated with a building, except in the area where such use abuts other service entrances or utility structures;
 - c. Loading docks or spaces, except in the area where such use abuts other loading docks or spaces;
 - d. Outdoor storage of materials, stock and equipment; and
 - e. Any other uses for which screening is required under these regulations;
4. **Landscape Buffer:** Any screening or buffer areas used to comply with the provisions of this section or other ordinance provisions for uses other than parking decks must consist of a planted area which is at least six feet (6'-0") wide.
 - a. This area may contain any type screening materials sufficient to separate visually the land uses, provided such materials meet the requirements of this section;
 - b. If only a wall or fence is used, then the area devoted to the screen need only be wide enough to accommodate the wall or fence and allow for its maintenance. The composition of the screening material and its placement on the lot will be left up to the discretion of the property owner, so long as the purpose and requirements of this section are satisfied;
5. **Installation Requirements:** The following contains standards to be used in installing screening:
 - a. Trees must be installed with a minimum 4" caliper and must be large deciduous or evergreen species which have a minimum growth height of 25'-0";
 - b. Trees should be trimmed up eight feet (8'-0") at the time of planting;
 - c. Shrubs used in any screening or landscaping must be evergreen, at least three feet (3'-0") tall with a minimum spread of two feet (2'-0") when planted and no further apart than four feet (4'-0"). They must be of a variety and adequately maintained so that an average height of three to four feet (3'-0" to 4'-0") could be expected as normal growth within four years of planting;
 - d. Any fence or wall used for screening shall be constructed in a durable fashion of brick, stone, other masonry material, specifically designed as fencing materials;
 - e. A chain link fence with plastic, metal or wooden slats does not satisfy the requirements of this section;

f. The maximum height for a wall or fence shall be four feet (4'-0") with the exception of screening for dumpsters which shall have a maximum height of six feet (6'-0");

6. **Off-Street Standards:** Buildings and structures, excluding parking structures, subject to the provisions of this section, must provide a minimum number of off-street service/delivery parking spaces. These spaces must be designed and constructed so that all parking maneuvers take place within the property line, service alley or secondary street. These parking spaces shall not interfere with the normal movement of vehicles and pedestrians in the public rights-of-way, unless otherwise approved.

These parking spaces must be provided in accordance with the following:

- Less than 25,000 square feet: None Required
- 25,000 - 50,000 square feet: 1 bay + 1 compactor
- 50,000 – 100,000 square feet: 2 bays + 1 compactor
- 100,000 – 150,000 square feet: 2 bays + 2 compactor
- Each additional 100,000 square feet: 2 bays + 1 compactor

For non-residential use with five (5) or more off-street service/delivery parking spaces, 40 percent of the spaces must be large enough to accommodate vehicles greater than 30 feet long.

Off-street loading and unloading shall be provided in such amount and manner that all loading and unloading operations will be conducted entirely within the boundaries of the lot concerned, and no vehicle shall use public streets, sidewalks or rights-of-way for loading or unloading operations, other than ingress and egress to the lot.

4.0: Development / Place-Making Design Standards:

The design standards in this section provide the criteria for existing and proposed development within the Redevelopment Area in order to promote a high quality, pedestrian friendly, mixed use environment. Any future development is subject to these provisions and should be built in accordance with the minimum design standards specified in this section. These standards promote:

- Appropriate scale for Town Center and the Redevelopment Area;
- Standards for commercial, office, residential and civic uses;
- Mixture of architectural styles with;

Town Center Design Standards:

1. **Architectural Character:**

All buildings shall reinforce pedestrian scale.

- a. The base of buildings shall be distinguished from the middle and top of the building with an emphasis on providing design elements that will enhance the pedestrian environment particularly at the street level;
- b. Elements such as cornices, belt courses, corbelling, molding, string courses, ornamentation, changes in material or color, and other sculpturing of the base are appropriate and should be provided to add special



Figure 4.0: Outdoor dining and streetscape



Figure 4.1: Architectural elements at the corner of buildings with vertical awnings



Figure 4.2: Varied building facade with corner elements

interest to the base;

- c. Special attention must be given to the design of windows at the base of buildings;
 - Ribbon windows are discouraged;
 - Recessed windows that are distinguished from the shaft of the building through the use of arches, pediments, mullions, and other treatments are encouraged;
- d. Building facades in excess of 120'0" in length shall be designed to avoid a monolithic appearance through the use of different façade materials and building setbacks that act to break the building appearance into smaller increments and sections;
- e. Building materials may include: brick, stone, cast stone, metal and glass storefront assemblies, wood and fiber cement siding;
 - Vinyl siding of any type and grade is strictly prohibited on any portion of any building façade visible from a public area within the Redevelopment Area;
 - First level commercial, retail and office may be designed using different material than the levels above;
- f. Primary building materials shall include: brick, stone, cast



Figure 4.3: Boulevard with angled and parallel parking

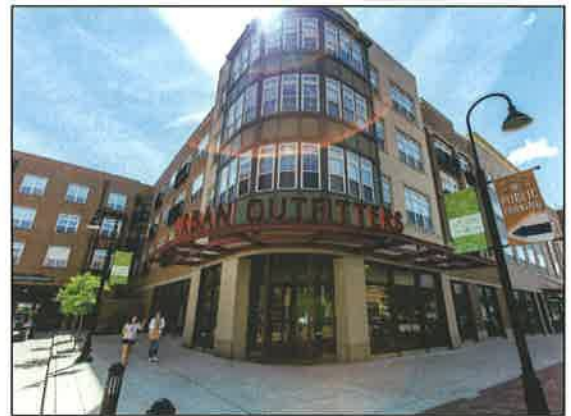


Figure 4.4: Architectural elements at the corner of buildings with vertical awnings



Figure 4.5: Corner elements with individualized entrances and streetscape

stone, and/or glass which cover a minimum of 65% of each building façade, exclusive of windows and doors with accent materials comprising a maximum of 35% for each building façade;

- g. Generic national branding architecture for freestanding retail / commercial buildings unless it meets the requirements of this section is prohibited;
- h. Storefront design should reflect the individual tenant's brand identity;
 - First level facades should be varied and avoid monolithic appearance;



Figure 4.6: Round-about with sidewalk for emergency access

2. **Building Orientation:**

All buildings shall be oriented toward public streets and public open spaces. The building front is considered to be the elevation facing any public street unless the building is adjacent to a park, plaza or open space, in which case the building should orient to both the public street and the open space.

- a. The first floors of all buildings, including structured parking, must be designed to encourage and compliment pedestrian-scale activity. It is intended that this be accomplished principally by the use of windows and doors arranged so that the uses are visible from and/or accessible to the street on at least 75% of the retail or commercial length of the first floor street frontage;



Figure 4.7: Boulevard with round-about and on-street parking



Figure 4.8: Buildings fronting Main Street should have retail on the first floor with unique and individual storefronts to provide variety.

- b. All new commercial and retail first floors shall have a 14'-0" minimum interior floor to floor height;

3. Building Entrances:

Building entrances should be easily identifiable and feature large, open and transparent windows with unique and interesting signage; (Operable windows are encouraged)

- a. Retail entrances can have up to a 2'-0" intrusion into the setback, allowing the 16'-0" setback to go to 14'-0" as long as the primary facade setback remains are 16'-0";
- b. Tenants may use this area for merchandising projection;
- c. Entrances for residential, office and uses other than retail should be separate and distinct from retail entrances;

4. Building Storefronts:

- a. All retail tenants can have the opportunity to design and install their own storefronts as a way to express their individual identity and positioning provided they observe the minimum guidelines noted below:
 - Storefronts should be "individual" expressions of a tenant's identity;
 - Tenant's storefront construction should be of high quality and craftsmanship;
 - Any restaurant use is encouraged to provide outdoor seating;
 - In locations adjacent to a public plaza, permanent shade structures for outdoor dining are strongly encouraged. The depth of outdoor dining should be coordinated based on the amount of area for pedestrian access;

5. Storefront Components:

- a. **Entries & Doors:** The placement of doors and their design are an integral part of the storefront because they establish a clear point of entry to the store;
 - Restaurants especially should use doors to open interior seating spaces to the outside café seating on the sidewalk;
- b. **Canopies & Awnings:** Canopies, awnings, and similar architectural accents are encouraged along the boulevard;



Figure 4.9: Outdoor dining with vertical rigid awnings



Figure 4.10: Ground level pop-outs



Figure 4.11: Streetscape with raised planters and seating

- Canopies and awnings should be comprised of rigid materials and should be horizontal in nature;
 - Any canopy may extend from the building up to one half of the width of the setback area in front of the building, or eight (8) feet, whichever is less;
 - Minimum overhead clearance shall be ten (10'-0") feet. If a canopy, awning, cornice, or other appurtenance extends into the public right-of-way, an encroachment agreement shall be required;
- c. **Balconies/balconettes:** Can be incorporated into the building façade and are allowed to project into the 16'-0" setback up to 4'-0" starting at the second level;
- d. **Windows:** Operable windows are strongly encourage for all restaurant uses facing the boulevard and public plaza's;

6. Storefront Materials:

- a. The approach to storefront design should be to create the highest quality level possible with an emphasis on user-friendly materials. The minimum recommendations below, outline some of the specifics when considering materials for storefronts:
- Focus should be on window design to create a visual connection between the interior and exterior;
 - Recommended materials are wood, metal, brick, stone, glass and concrete as well as plaster;
 - Acceptable materials are durable, smooth exterior grade woods such as oak, redwood and poplar;
 - Durable materials are especially critical at street level where pedestrian contact will be considerable. EIFS or similar materials are not permitted on the ground level;
 - Storefronts should be predominantly glass to provide views into the store, but glass should not be the exclusive material;
- b. The following materials are strongly discouraged:
- Plastic and metal laminates;
 - Acrylic;
 - Plastics;



Figure 4.12: Corner elements with vertical rigid awning



Figure 4.13: Landscape median



Figure 4.14: Outdoor seating fronting public plaza

- Smoked or tinted glass;
- Simulated materials;
- Mirror;
- EIFS;

7. **Street Level Frontage / Uses:**

Uses fronting the boulevard must be either occupied commercial, retail, entertainment or restaurant.

- a. Residential uses are not permitted fronting on the first floor along the boulevard but are permitted on the first floor for all other streets in the Redevelopment Zone;
 - Residential lobbies and entrances, however are permitted on the first floor along the boulevard so long as they are less than 25% of the total street frontage for the building;
 - Entrances for residential should be distinct and separate from the entrances of all other first floor uses;
- b. All non residential ground floor space shall include a ventilation shaft regardless of whether or not restaurants uses are proposed;
- c. Temporary / semi-permanent outdoor dining and seating is encouraged within the minimum 16'-0" setback for retail and restaurant uses;
- d. Seating is encouraged to be designed either along the building façade or at the back of the curb. These areas should be clearly identified with either permanent, semi-permanent or temporary barriers;
- e. A minimum six foot (6'-0") clear zone must be maintained within the building setback in order to allow adequate pedestrian flow;

11. **Doors and Windows:**

Where windows are used they must be transparent. Where expanses of solid wall are necessary, they may not exceed 30'-0" feet in length.

- a. The first floor and street level must be designed to address all public streets and all adjacent public plaza;
- b. For buildings fronting on two streets or one street and one public plaza or open space the entrance to the



Figure 4.15: Multi-functional outdoor plaza that permits ice skating in the winter months.



Figure 4.16: Multi-functional outdoor plaza that permits seating and outdoor dining in the spring and summer months



Figure 4.17: Plaza with tivolli lighting

building should provide access from the primary street and is encouraged to provide access from the secondary street or public plaza;

- For buildings that do not provide access from the secondary street or open space, the building facade should provide windows and/or architectural details that are aesthetically interesting;

- c. Operable windows are strongly encouraged at the street level;

12. Public Plaza:

- a. For any building that fronts a public plaza, the first floor uses shall wrap the building and provide frontage on the plaza space;
- b. The public plaza shall be designed as a multi-functional space that permits a variety of uses through the year for gathering;

13. Structured Parking:

- a. Structured parking shall be screened with uses along the boulevard;
- b. Structured parking shall be screened with either uses or shall include decorative architectural details such as building materials, windows and detailing along all other streets in the Redevelopment Area;
 - Architectural detail elements should have the same or similar materials and should have the same or similar scale for openings as the residential / commercial building above;
 - Other than at the parking entrances, vehicles shall not be visible from the center line of either public street;
- c. All ramps shall be internal to the parking structure and shall not be visible from any public street;
- d. Driveway and garage openings should not exceed 28'-0" in width and should include traffic calming measures and a change in surface materials where driveways cross the sidewalk to help ensure pedestrian and bicycle safety;



Figure 4.18: Plaza with outdoor seating areas



Figure 4.19: Plaza with outdoor seating areas



Figure 4.20: Structured parking deck with architectural elements

14. Mechanical Equipment Screening:

The screening of rooftop mechanical equipment is required.

- a. All rooftop mechanical equipment including cell phone antennae shall be screened from view from all adjacent public streets, open spaces and parks in all directions and elevations to minimize the negative impact from any public street, neighborhood or adjacent building;
- b. Screening materials shall be consistent with the architectural detail, color and materials of the building;
 - Wire mesh screening is not permitted;
- c. All roof and HVAC systems must be set back a minimum of 15' from the boulevard and 10' from any other public street or public open space from the building edge and screened as to not be visible from any adjacent public street or public property;
- d. Any wall pack ventilation unit facing a public street must match the adjacent material color;

15. Building Service Locations:

All service locations for new development shall be provided at the rear, side or interior portions of the building and shall not be permitted directly on the boulevard;



Figure 4.21 Streetscape with permanent outdoor seating areas



Figure 4.22: Plaza with street closure during large events

ROUTE 18 - EAST BRUNSWICK REDEVELOPMENT PLAN



Figure 4-23: Perspective rendering of the Route 18 Redevelopment Plan



5.0: Streetscape Design Standards:

A successful neighborhood is not complete until its parks, open space, sidewalks and streetscape have been designed and “furnished”. The items that are referred to as streetscape elements in this guideline include such things as street lights, sidewalks, benches, trash receptacles and other street furniture which reinforce the character of the street and the neighborhoods. The following section addresses an overall approach for the design of streetscape elements that are to be considered as part of the overall Redevelopment Plan vocabulary. These standards are meant to establish the minimum criteria that will be required for the design and implementation of streetscape, parks and open space improvements.

As a part of the revitalization of the Redevelopment Area, East Brunswick Township will develop specific streetscape standards for Main Street and the other streets in the Redevelopment Area. All new buildings and developments in the Redevelopment Area, with the exception of renovated and rehabilitated buildings, shall meet the following minimum standards.

1. **Town Center Streetscape:** The streetscape for the Redevelopment Area should be designed with a similar palette of materials and standards in order to portray a cohesive district.
2. **Street Dimensions:**
 - a. The minimum setback from the building to the face of curb shall be 16'-0”;
 - b. The boulevard should be based on a paired one way system;
 - c. Parallel parking may be provided along the 16'-0” setback;
 - d. Pedestrian bump outs may be included at all internal intersections;
3. **Streetscape Elements:** The following streetscape elements shall be provided per this section of the Design Standards. Proposed development projects shall provide these elements as a part of the approval process;
 - a. **Street Trees:** Street trees shall be planted in either grates or open landscape areas equivalent to 40'-0” on center along all public street frontage;
 - Street trees shall be planted with a minimum 3.5” caliper, shall be trimmed up to 8'-0” and shall be in accordance with the “American Standard for Nursery Stock” published by the American Association of Nurserymen;
 - Prior to installation a minimum 4'-0” by 8'-0” area should be treated and prepared for tree root growth.
 - A minimum 2 year maintenance and watering plan shall be put into place;
 - Recommended street tree: Gleditsia Tricanthos (Honey Locust – thornless and pod free), Zelkova, Katsura, Hedge Maple, and Hornbeam or other trees approved by the board;
 - 120 V ground mounted GFI receptacles should be located at the base of every street tree;
 - Tree up light fixture may be located at the base of every street tree. (Recommend: Hadco #BT1-AL, 75 W MR16 Lamp or equal);
 - Conduit for all power may be located at the back of the 4'-0” street furniture area (under the pavers);
 - Drip irrigation system and/or micro-sprayers;
 - b. **Design & Materials:**

- Sidewalks should have a higher level of finish on pedestrian oriented, mixed-use streets (pavers, colored stamped concrete, etc. - asphalt is prohibited);
 - The pattern and type of materials will be determined during the design phase;
- c. **Seating, Benches & trash receptacles:**
- **Benches or other seating features and trash receptacles should be located along the street to provide for seating areas at a recommended 1 bench per 120'-0" on average.**
- d. **Street Lighting:**
- Street lights should be located as part of the streetscape and function as a unifying element;
 - Fixtures should exhibit an aesthetic as well as functional purpose to create interest and a sense of scale for the pedestrian and should provide at minimum brackets to allow for banners;
 - Luminaires should be translucent or glare-free using opaque glass or acrylic lenses;
 - Diffusers and refractors should be installed to reduce unacceptable glare adjacent to residential areas;
 - The specific street light and spacing of poles shall be determined during the design phase;
 - GFI's should be located at the top of the pole;
 - The height and spacing of the light poles should be based on a photometric calculation;
 - Lights should be centered between the street trees within the 4'-0" street furniture zone;
 - Street light specifications and locations shall be submitted for review and approval prior to installation;
- e. **Planting Pots & Planters:**
- Pots and planters can be used in addition to landscape planting areas to complement the surrounding streetscape by adding color and variety;
 - Pots and planters can be placed anywhere



Figure 5.1: Outdoor dining along the building and back of curb



Figure 5.2: Street with paver furniture zone along the back of curb



Figure 5.3: Street with change in material to delineate furniture zone

pedestrian or vehicular traffic is not disturbed, but may function as a separation between pedestrians and vehicles;

- Large pots are preferred to fixed boxes to allow for maintenance or service access;
- Pots and planters are ideally located near seating areas, but plant materials should not interfere with circulation or comfort;
- Drip irrigation system;
- Flower baskets;

f. Tree Grates:

- Tree grates can be used wherever a tree is placed within a high traffic area;
- Tree grates should allow for tree growth and be made of ductile iron and should be factory painted;
- Tree grates should be designed to allow for GFI's and uplighting;

g. Trash Receptacles:

- Trash receptacles should be located conveniently for pedestrian use and service access in significant areas and gathering places;
- Trash receptacles should be permanently attached to deter vandalism and have sealed bottoms with sufficient tops to keep contents dry and out of pedestrian view;
- The type and location of the benches will be determined during the design phase;
- At minimum every other trash receptacle should include an additional recycling container;

h. Bicycle Racks:

- Bicycle racks should be permanently mounted and placed in convenient locations to encourage bicycle use but not to obstruct views or cause hazards to pedestrians or drivers;
- Bicycle racks should exhibit a simple and easy design that allows for convenient and safe use by the public and should not be placed on within the 18'-0" building setback;

i. Bollards:



Figure 5.4: Streetscape with landscape planting area and outdoor dining

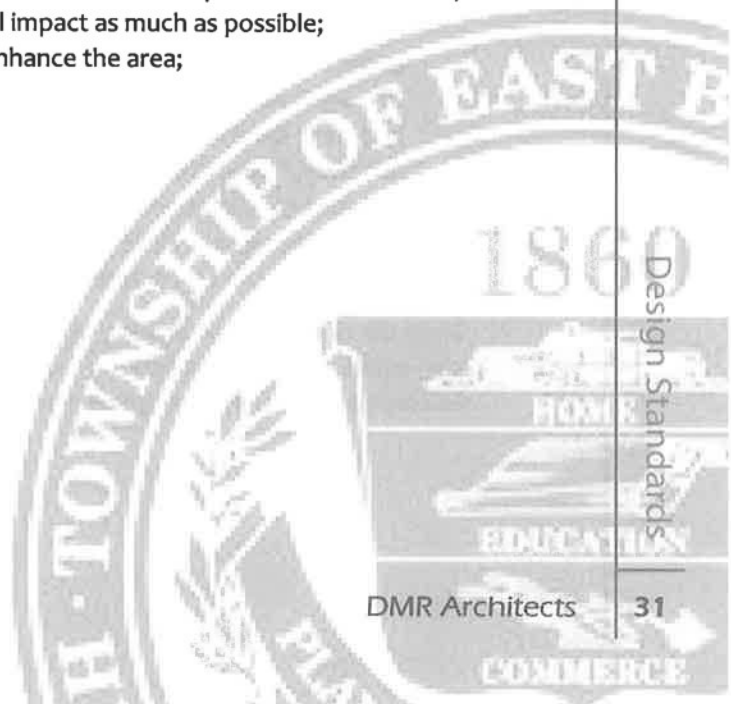


Figure 5.5: Outdoor dining along the building and back of curb



Figure 5.6: Entry monument at primary entrance

- Bollards should integrate with and aesthetically complement the overall streetscape concept; respond to the area it supports (Plaza vs Services); setback from curbs to allow unobstructed opening of parked car doors;
 - Bollards may be chained or cabled together to ensure pedestrian safety or define areas for public functions;
 - Removable bollards should be used where service vehicles need access and for street closures in the event of festivals or community events;
- j. Fountains:
- Fountains or water features can be located in key public areas such as parks or plazas, should allow for ample pedestrian circulation on all sides and should be designed to encourage “sitting” or for interaction;
- k. Public Art:
- Public art should be “accessible”; tie to the history of East Brunswick and if possible, should be created by a local source; include water, seating, planting, decorative architectural elements or plaza space design;
 - Public art should be visible, but not interfere with pedestrian circulation or create a traffic hazard; made of durable, weatherproof materials; and should be designed to avoid physical hazards;
- l. Kiosks:
- Kiosks can be used for retail purpose or to impart community information to the public;
 - Kiosks should be accessible and attractive from all sides and well-illuminated and they should be flexible to allow for up-to-date information;
- m. Utility Accessories:
- Utility boxes, meters, man hole covers and fire hydrants should be coordinated with other streetscape accessories;
 - Utilities should be readily accessible and placed so as not to obstruct pedestrian movement;
 - Utility locations should minimize visual and physical impact as much as possible;
 - Utilities should blend in with the surroundings or enhance the area;



6.0: Signage Standards:

1. Signage Standards Overview:

- a. The developer is encouraged to explore a variety of signage types, sizes and styles with the objective of integrating the design of the signage into the streetscape design. Storefronts are expected to take maximum advantage of store logos, specialty letter styles, quality materials, graphic flourishes and high impact graphics;
 - The goal is to maintain creative consistency that identifies the tenant's identity and integrates with the storefront façade design;
 - Signs can be in the form of a painted sign, flat sign, fin sign, window sign, illuminated or non-illuminated sign, dimensional sign, sidewalk sign or awnings;
 - Emphasis should be on durable, natural materials and quality manufacturing and can include cast, polished or painted metal; painted, stained or natural wood; glazed and ceramic tile; etched, cut, edge-lit or stained glass; cast stone and carved natural stone;
 - Tenants should strive for creativity, uniqueness and high quality;
 - Neon signs strictly prohibited;
- b. For all signage standards not covered or specifically stated in this Section, refer to Article XXXI - Signs (228-244 through 228-257) with the exception of the following;
 - One additional Pylon Sign (located at "Road N") similar in size (460 SF per face) and design as the approved pylon sign from Phase 1.
 - Four ground/monument signs similar in size (230 SF) as the approved sign from Phase 1.
 - One sign proposed at each of the following locations: the Ruth Street driveway, the "Road F" driveway, "Road N & Boulevard" and one at the end of Renee Road to the townhouse area.
 - Wayfinding signage (8 SF per face); number of signs shall be sufficient for safe & proper circulation.
- c. Given the nature and complexity of this redevelopment plan additional bulk variances may be required and should be considered as part of a signage package by phase.



Figure 6.0: Pylon Sign Typ.



Figure 6.1 - Monument Sign Typ.

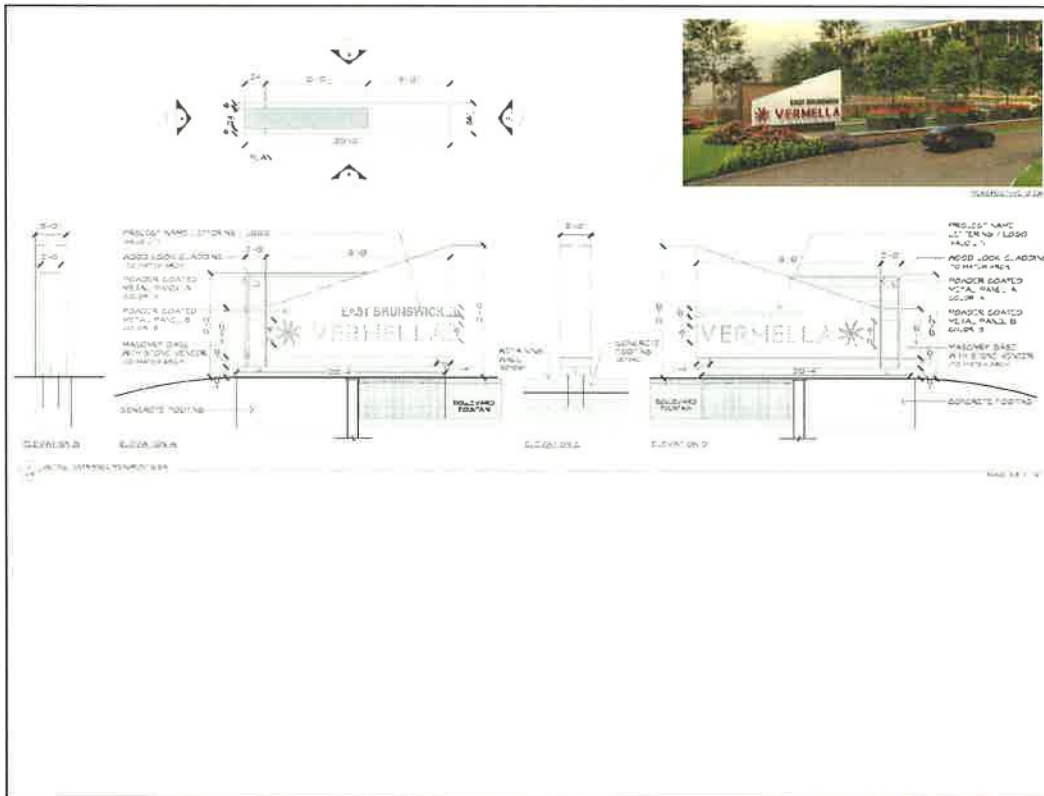


Figure 6.2: Monument Sign Typ.

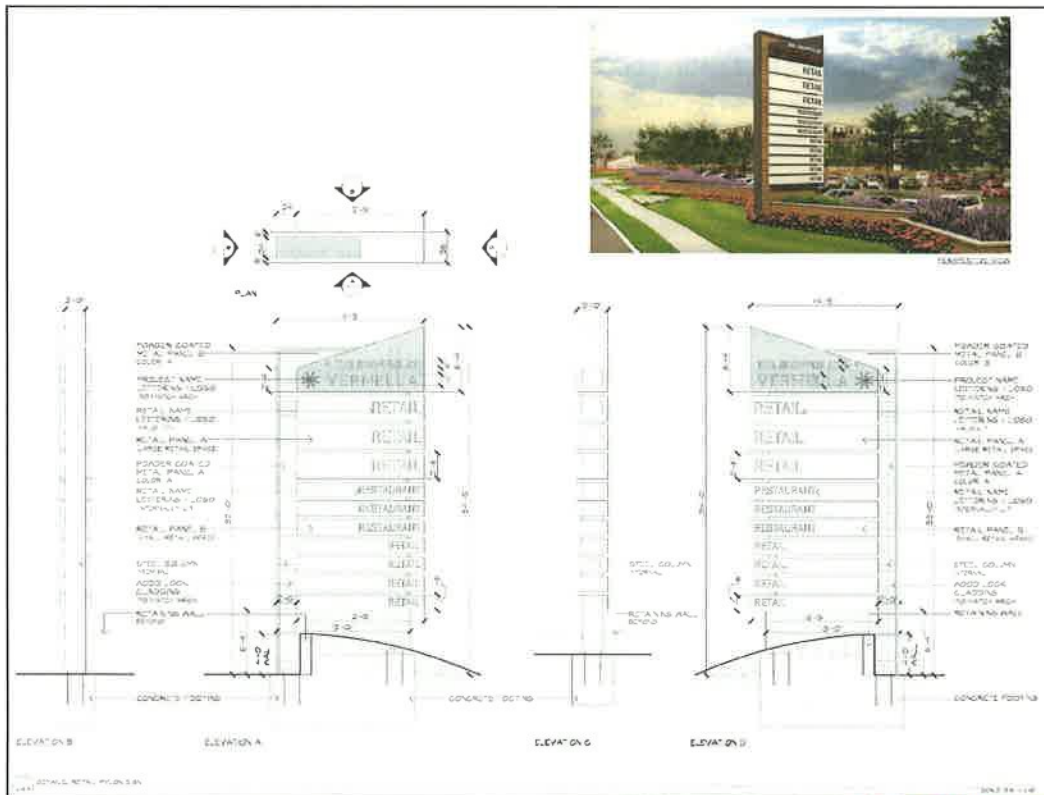


Figure 6.3: Pylon Sign Typ.

7.0: Relationship to the State / County and Local Master Plans:

A. State Development and Redevelopment Plan (SDRP):

In reviewing the New Jersey State Development and Redevelopment Plan Volumes 1 - 4 the following information pertains to goals and policies for a program of rehabilitation which discuss the development and redevelopment policies for urban areas.

1. Volume II – State Plan Goals and Policies include the following:
 - a. Revitalize the State's Cities and Town Centers: Revitalize New Jersey's cities and towns by investing wisely and sufficiently in improvements in their infrastructure systems, public spending programs, tax incentives and regulatory programs to leverage private investment and to encourage infill and redevelopment in ways that are consistent with the State Plan's vision and goals.
 - b. Conserve the State's Natural Resources and Systems: Conserve the State's natural resources and systems by planning the location and intensity of growth to maintain natural resource and systems capacities and make the necessary infrastructure investments to protect natural resources and systems in ways that guide growth and development in ways that are consistent with the State Plan's vision and goals.
 - c. Promote Beneficial Economic Growth: Promote beneficial economic growth in locations and in ways that improve the quality of life and the standard of living for all New Jersey residents by providing infrastructure in advance of, or concurrent with, the impacts of new development sufficient to maintain adequate facility standards, by encouraging partnerships and collaborative planning with the private sector and by capitalizing on the State's strategic location, economic strengths including its existing business enterprises, entrepreneurship, the research and development capacity of its institutions of higher learning, skilled workforce, cultural diversity and logistic facilities in ways that are consistent with the State Plan's vision and goals.
 - d. Protect the Environment, Prevent and Clean up Pollution: Protect the environment, prevent and clean up pollution by planning for growth in compact forms at locations, densities and intensities that protect land, air and water quality, allow expeditious regulatory reviews and encourage multi-modal transportation alternatives to the automobile to help achieve and maintain acceptable air quality standards.
 - e. Provide Adequate Public Facilities and Services at Reasonable Cost: Provide adequate public facilities and services by supporting investments based on comprehensive planning and by providing financial incentives for jurisdictions that cooperate in providing public infrastructure and shared services. Encourage the use of infrastructure needs assessments and life-cycle costing. Provide adequate public facilities in ways that are consistent with the State Plan's vision and goals.
 - f. Provide Adequate Housing at Reasonable Cost: Provide adequate housing at reasonable cost through public/private partnerships that create and maintain a full range of attractive, affordable, and environmentally sensitively-designed and developed housing, particularly for those New Jersey State Development and Redevelopment Plan most in need, at densities and locations that provide greater efficiencies and serve to support public transportation alternatives and reduce commuter time and expense and easily accessible to employment, retail, cultural, civic and

recreational opportunities to reduce housing and commuting costs in ways that are consistent with the State Plan's vision and goals.

- g. **Preserve and Enhance Areas with Historic, Cultural, Scenic Open Space, and Recreational Value:** Preserve, enhance, and use historic, cultural, scenic and recreational assets by collaborative planning, design, investment and management techniques. Locate and design development and redevelopment and supporting infrastructure to improve access to and protect these sites. Support the important role of the arts in contributing to community life, civic beauty and redevelopment in ways that are consistent with the State Plan's vision and goals.
 - h. **Ensure Sound, Coordinated and Integrated Statewide Planning:** Ensure sound, coordinated and integrated statewide planning by using the State Plan as a guide to planning and growth related decisions at all levels of government in ways that are consistent with the State Plan's vision and goals.
 - i. **Increase Energy Efficiencies and Reduce Greenhouse Gas Emissions:** Increase Energy Efficiencies and Reduce Greenhouse Gas Emissions by promoting the improved coordination and integration of transportation planning and land-use planning and decision-making to reduce vehicle miles traveled (VMT); and by the citing, development, design and use of green-building construction materials and techniques in ways that are consistent with the State Plan's vision and goals.
2. The State Planning Act (N.J.S.A. 52:18A-196 et. seq.) states:
- a. "Among the goals of the act is the following: ... conserve its natural resources, revitalize its urban centers, protect the quality of its environment, and provide needed housing and adequate public services at a reasonable cost while promoting beneficial economic growth, development and renewal."
 - b. "It is in the public interest to encourage development, redevelopment and economic growth in locations that are well situated with respect to present or anticipated public services and facilities, giving appropriate priority to the redevelopment, repair, rehabilitation or replacement of existing facilities and to discourage development where it may impair or destroy natural resources or environmental qualities that are vital to the health and well-being of the present and future citizens of this state." (N.J.S.A. 52:18A-196 (d))
3. The State Plan Policy Map (SPPM)
- a. Recognizes that New Jersey requires different approaches in its Metropolitan, Suburban, Rural and Environmentally Sensitive Planning Areas. The New Jersey State Development and Redevelopment Plan adopted by the State Planning Commission, places East Brunswick Township in Planning Area 1 - the Metropolitan Planning Area and states:
 - b. The Metropolitan Planning Area: This Planning Area includes a variety of municipalities that range from large Urban Centers to 19th century towns shaped by commuter rail and post-war suburbs. The Communities in this Planning Area have strong ties to major metropolitan centers – New York/Newark/Jersey City metropolitan region; the Philadelphia/Camden/Trenton Metropolitan Region; and on a smaller scale the Easton/Phillipsburg Metropolitan Region.
 - c. These municipalities have many things in common: mature settlement patterns; infrastructure systems that are approaching their reasonable life expectancy; an aging housing stock in need

of rehabilitation; recognition that redevelopment will be the predominant form of growth; and a growing realization of the need to regionalize services and systems. In addition, the wide and often affordable choice of housing in proximity to New York and Philadelphia has attracted significant immigration, resulting in noticeable changes in demographic characteristics overtime.

- d. In the Metropolitan Planning Area, the State Plan's intent is to do the following:
- Provide for much of the State's future development and redevelopment;
 - Revitalize Cities and Towns;
 - Take advantage of increased densities and compact building design;
 - Encourage distinctive, attractive neighborhoods with a strong sense of place;
 - Provide for mixed-use concentrations of residential and commercial activity;
 - Create a wide range of residential housing opportunities and choices with income mix;
 - Provide for a variety of multi-modal transportation alternatives;
 - Prioritize clean-up and redevelopment of brownfields and greyfields sites;
 - Create cultural centers of state-wide significance;
 - Re-design any existing areas of low-density sprawl;

B. 2005, 2011 and 2015 Master Plan Reexamination Report:

In reviewing the 2015 Master Plan Reexamination Report for the Township of East Brunswick, the following information pertains to goals and policies for this redevelopment plan.

- a. To continue to meet the housing needs of wide range of income and age levels; (2005 - 2011 - 2015)
- b. To continue to provide varied housing types in meeting these needs; (2005 - 2011 - 2015)
- c. To utilize planning and development techniques which consider social and environmental factors in meeting future housing needs; (2011 - 2015)
- d. To locate higher density housing near public transportation; (2005 - 2011 - 2015)
- e. To continue to recognize the role of Route 18 as a major regional commercial retail area, but to continue to tightly define the extent of commercially zoned land along Route 18 to minimize adverse impacts on nearby residential areas and to improve the quality of exiting commercial development along the highway; (2005 - 2011 - 2015)
- f. C-1 Zones: The C-1 zone is a very limited office zone with R-3 bulk standards recommend deleting the C-1 zone in its entirety and rezoning properties along Route 18 to HC-2 were appropriate and mostly to HC-1 and along Cranbury Road to OP-1, O-1 and R-2. The HC-1 zone has greater lot size and setback requirements than the C-1 zone, however most of the existing C-1 properties are non conforming; (2011 - 2015)
- g. To encourage additional tax producing uses in appropriate locations, such as office facilities, to assist in meeting local fiscal needs to provide professional support services for the residents of the township and to provide additional local job opportunities; (2005)
- h. To continue to evaluate existing development controls regarding design standards, lot sizes, building setbacks, coverage and density of various land uses, in order to ensure compatible development with the existing environment and nearby developed areas; (2005)

- i. To address the particular problems involved in establishing appropriate development standards for lots which adjoin high density residential and commercial areas; (2005)

7.1: Neighboring Community Master Plans:

Information for the Master Plans of the adjacent municipalities which includes the following information:

1. **North Brunswick:** Two segments of North Brunswick adjoin the township. They lie north and south of Milltown. Both segments are separated from the township by bodies of water, Farrington Lake to the south and Weston's Mill Pond to the north. With the exception of Ryders Lane crossing Weston's Mill Pond and two other roads crossing Farrington Lake, there is no contact between the two townships. The water serves as an effective separator of zoning impacts.
2. **New Brunswick:** A small portion of New Brunswick adjoins the township in the vicinity of Route 18 and the Turnpike. As with North Brunswick, there is a physical separation of land use between the municipalities related to Weston's Mill Pond and Lawrence Brook. The road contact is with Route 18, which is fully developed in both communities, and Burnet Street, which functions as a part of the access to the Tower Center office buildings and plays a role in providing access to the Turnpike interchange from the adjoining New Brunswick. There are no land use conflicts in this area even though the zoning is different on both sides of the boundary. The absence of conflict is based on the physical separation between the different zoning districts and is further based on the fact that the area is essentially fully developed.
3. **Edison:** Edison is separated from East Brunswick by the Raritan River and there are no roads crossing from one township to the other. The zoning in Edison is industrial, which is the same as that found in East Brunswick.
4. **Sayreville:** Sayreville adjoins the township in two areas lying both north and south of the South River. The body of water known as the South River separates both sections of East Brunswick. The only area where there is land use contact lies along Main Street where the zoning in Sayreville is multi-family residential and the corresponding zone in the township is Historical Residential and Commercial. This is not a land use conflict since the South River serves as an effective boundary of the zoning districts, and any multifamily development would relate well to the commercial and historic zoning districts in the township.
5. **South River:** South River has the most direct contact with land uses in East Brunswick of all adjacent municipalities. Most of the contact lies along Old Bridge Turnpike. For the most part, the zoning districts along Old Bridge Turnpike relate well to each other. Most of the frontage in South River is zoned for residential. The areas where there are differences in zoning include:
 1. Tices Lane where East Brunswick has commercial zoning and South River has residential lying south of Tices Lane.
 2. Milltown Road, where South River has commercial zoning and East Brunswick has residential.
 3. Just north of Milltown Road in East Brunswick, the township has commercial zoning opposite residential zoning in South River. This is the section of the township where Old Bridge Turnpike and Route 18 are in closest proximity.

In addition to the conflicts identified above along Old Bridge Turnpike, there is another zoning

difference on River Road where South River has industrial zoning adjacent to residential zoning in East Brunswick.

6. **Old Bridge:** While there is some road contact between Old Bridge and East Brunswick, for the most part the townships are effectively separated by the South River. Based on field observation, most of the conflict in planning between the two municipalities relates to the traffic tie ups at the traffic light just east of Route 18 at Main Street and Old Bridge Turnpike.
7. **Sportswood:** For the most part, zoning in Sportswood is compatible with that found in East Brunswick. The small commercial district along Old Stage Road in Sportswood lies opposite a small retail commercial district in the townships. The only apparent land use conflict is in the small industrial area located near the Helmetta border, but it adjoins an area in East Brunswick which is owned by Middlesex County as public land.
8. **Helmetta:** Most of the Helmetta border is zoned differently than the land lying in East Brunswick. This difference in zoning approach is not a basic conflict for the main reason that the land in East Brunswick is owned by the county. However, full development of land in Helmetta as industrial could bring with it added traffic on Helmetta Boulevard and potential problems at the intersections of Helmetta Boulevard and Old Stage Road, and further west at the intersection of Cranbury Road.
9. **Borough of Milltown:** The Borough is located northeast of the municipal boundary for East Brunswick. Along the municipal boundary Milltown is developed with a combination of residential single-family homes and a shopping center with major big box retailers. The Borough is developed mostly with residential development with very little new development in the past several years. The Borough of Milltown will not be negatively affected by this redevelopment plan.
10. **Monroe Township:** The Township is located south of the most southern part of East Brunswick. The land directly adjacent to the East Brunswick border is Jamesburg Park, this open space continues into the Township of East Brunswick and is compatible with the zoning in both municipalities. In addition, the Township has experienced more development than any other town in the county in the past 15 years. Much of this development is a mixture of housing types similar to this Redevelopment Plan. The plan will not have any detrimental effect on the Township of Monroe.
11. **South Brunswick Township:** The land located along the municipal boundary is primarily composed of land zoned for Public Land and Rural Residential and is zoned similarly on the East Brunswick side of the municipal boundary. The township has not reported any major development since 2011 and a majority of the land use within the South Brunswick is residential. This Redevelopment Plan will not negatively impact the goals of South Brunswick Township.

7.2: Temporary and Permanent Relocation:

The Local Redevelopment and Housing Law requires that any redevelopment plan make adequate provision for temporary or permanent relocation of any resident in the project area. As there are no residential buildings in the redevelopment area, there is no need to make provision for temporary or permanent relocation of any resident.

7.3: Identification of Property to be Acquired:

The Local Redevelopment and Housing Law requires that any redevelopment plan identify any property within the redevelopment area which is proposed to be acquired in accordance with the redevelopment plan. While it is the Township's fervent hope that a private developer will obtain the necessary property interests to privately develop the entire redevelopment area; the Township does not rule out acquiring each and every property in the redevelopment area in order to redevelop the area in accordance with the goals and objectives stated in this redevelopment plan.

7.4: Affordable Housing Obligation:

As of the date of adoption of the resolution finding the area to be in need of redevelopment, no affordable housing units are to be removed from the Township's inventory as a result of the implementation of this redevelopment plan. However, the Township of East Brunswick does have an obligation to provide affordable housing units. As such the developer shall either construct affordable units within the project or contribute to the Township's trust fund to assist the Township in meeting its obligation, which will be determined as a part of a redeveloper's agreement.

7.5: Project Signage:

The redeveloper shall erect semi-permanent signage at locations to be determined by the Township within 30 days of receiving approval from the Planning Board that contains a rendering or renderings of the finished project for marketing purposes.

7.6: Traffic Impact Analysis:

A traffic impact analysis should be provided as a part of any site plan submission to the Planning Board. The analysis should forecast additional traffic associated with the entire development, determine vehicular and pedestrian improvements necessary to accommodate the proposed development to help ensure safe and reasonable traffic conditions and reduce the negative impacts created by the development.

7.7: Long Term Financing Considerations:

Based on the nature and size of the potential projects contemplated under this plan, the Township of East Brunswick anticipates consideration for long term tax abatements (PILOT's) if the project commences within 36 months of plan adoption. After that time, the specific economic conditions would have to be reevaluated to determine the duration of an exemption that would be deemed financially feasible and appropriate. The actual entry of any financial agreements for a long term tax financing are subject to governing body approval under the processes required by law. The designated developer will be required to provide a preliminary fiscal impact analysis outlining the existing and proposed positive and negative impacts of the proposed development on municipal services.

In order to promote a transparent process, the Township of East Brunswick would like to set forth the parameters and guidelines for long term tax abatements for private development projects within this Redevelopment Plan. Factors that will be considered include having adequate tax revenues to support essential city services, creating jobs and fostering the right mix of housing, commercial and recreational opportunities necessary to cultivate and strengthen the Township as a whole and the surrounding areas.

The policy guidelines will utilize the Long Term Abatement Law to encourage redevelopment in and surrounding the downtown, while recognizing the process must be transparent, predictable and within the parameters of state law. All long-term tax abatement agreements must be individually negotiated between the municipality and the designated redeveloper. Long-term tax abatements can only be adopted by ordinance after appropriate public notice and a public hearing is conducted. Long-term tax abatements must be monitored by the Township on an annual basis. By law, in no circumstances can the payment be less than the last full year of conventional taxation.

This policy includes a three (3) tiered approach based on the project size, status and location. For any project seeking long-term tax exemptions outside of the parameters of this policy, the Township may consider long term tax abatements based on the criteria that most closely represent the project type, size and impact.

Tier One Projects:

Applicable Criteria:

Project Area	Must be an Area in Need of Redevelopment
Project Type	Mixed-use multi-family rentals
Min. Investment	\$60 million capital investment and/or 240 units
Max Term	30 years
Annual Service Charge	To be negotiated.
Admin Fee	To be negotiated.
Other Requirements	Requires entry into Redevelopment Agreement and compliance with all applicable ordinances.

Tier Two Projects:

Applicable Criteria:

Project Area	Must be an Area in Need of Redevelopment
Project Type	Mixed-use multi-family rentals
Minimum Size	\$30 million capital investment and/or 180 units
Max Term	20 years
Annual Service Charge	To be negotiated.
Admin Fee	To be negotiated.
Other Requirements	Same as Tier One

Tier Three Projects:

Applicable Criteria:

Project Area	Must be an Area in Need of Redevelopment
Project Type	Mixed use multi-family rentals
Minimum Size	\$15 million capital investment and/or 100 units
Max Term	15 years
Annual Service Charge	To be negotiated.
Admin Fee	To be negotiated.
Other Requirements	Same as Tier One